

GENERAL NOTES

1. THE TYPICAL DETAILS DEPICTED ON THE STANDARD SHEETS AND IN THE MUTCO, REFLECT THE MINIMUM REQUIREMENTS.
2. THE CONTRACTOR MUST SUBMIT TO THE ENGINEER, IN WRITING, PROPOSED REVISIONS TO THE TRAFFIC CONTROL PLAN FOR REVIEW AND APPROVAL BY THE REGIONAL DIRECTOR OR HIS/HER DESIGNEE. SUCH REVISIONS MUST BE SUBMITTED WITHIN THE TIME FRAME OF THE PROPOSED RETAINERS, EXCEPT FOR CHANGES THAT ALTER THE SCOPE OF THE TRAFFIC CONTROL PLAN. SUCH CHANGES IN SCOPE MUST BE SUBMITTED TO THE ENGINEER FOR APPROVAL BY THE REGIONAL DIRECTOR OR HIS/HER DESIGNEE THIRTY (30) WORKING DAYS PRIOR TO IMPLEMENTATION OF SUCH REVISIONS.
3. THE CONTRACTOR SHALL PROVIDE THE ENGINEER, IN WRITING, WITH THE NAMES, PHONE NUMBERS AND ADDRESSES OF ALL PERSONS AND FIRMS EMPLOYED BY THE CONTRACTOR WHO WILL PROVIDE THE TRAFFIC CONTROL SERVICES. THE LABOR CONTRACTOR SHALL PROVIDE THE ENGINEER WITH THE NAMES, PHONE NUMBERS, ADDRESSES, AND EQUIPMENT FOR EMERGENCY REPAIRS OUTSIDE NORMAL WORKING HOURS. THE ENGINEER WILL PROVIDE THE SUBMITTED INFORMATION TO REGIONAL MANAGEMENT, THE NEW YORK STATE POLICE, THE RESIDENT ENGINEER, AND THE LOCAL POLICE.

ACTIVITY AREA

1. THE CONTRACTOR SHALL MAINTAIN A MINIMUM SIX (6) LONGITUDINAL DISTANCE BETWEEN CONSTRUCTION OPERATIONS ON ALTERNATE SIDES OF THE ROADWAY, UNLESS OTHERWISE APPROVED BY THE ENGINEER.
2. WHEN TWO OR MORE AREAS ARE ADJACENT, OVERLAP, OR ARE IN CLOSE PROXIMITY, THE CONTRACTOR SHALL ENSURE THERE ARE NO CONFLICTING SIGNS AND THAT LANE CONTINUITY IS MAINTAINED THROUGHOUT ALL WORK AREAS.

SIGNS

1. THE LOCATIONS OF THE SIGNS SHOWN ON THE WORK ZONE TRAFFIC CONTROL PLAN AND DETAILS MAY BE ADJUSTED BASED ON SIGHT DISTANCE AND OTHER CONSIDERATIONS. THE FINAL LOCATIONS OF SIGNS ARE SUBJECT TO APPROVAL OF THE ENGINEER.
2. ANY EXISTING SIGNS, INCLUDING OVERHEAD SIGNS, WHICH CONFLICT WITH THE TEMPORARY TRAFFIC CONTROL SIGN LAYOUT SHALL BE COVERED, REMOVED, STORED OR RESET, AS APPROVED BY THE ENGINEER. ALL APPROPRIATE EXISTING SIGNS SHALL BE RESTORED TO ORIGINAL CONDITION AND/OR LOCATION UNLESS OTHERWISE REPLACED IN THIS CONTRACT.
3. SIGNS AT OR NEAR INTERSECTIONS SHALL BE PLACED SO THAT THEY DO NOT OBSTRUCT A MOTORIST'S LINE OF SIGHT.
4. ALL WARNING AND REGULATORY SIGNS SHALL BE POSTED ON BOTH SIDES OF MULTI-LANE DIVIDED HIGHWAY WORK ZONE. ALL REGULATORY SIGNS SHALL BE PLACED ON THE RIGHT SIDE OF THE TRAVEL LANE TO ONE LANE. SIGNS SHALL BE POSTED ON THE RIGHT SIDE OF THE TRAVEL LANE, UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER.
5. SIGNS MOUNTED ON THE MEDIUM OF DIVIDED HIGHWAYS WHERE MEDIUM BARRIER IS IN PLACE SHALL BE MOUNTED ON THE MEDIUM BARRIER. SIGNS MOUNTED ON THE MEDIUM BARRIER IN A HORIZONTAL POSITION IS NOT PERMITTED.
6. THE DIMENSIONS OF WORK ZONE TRAFFIC CONTROL SIGNS ARE DESCRIBED IN THE MUTCO. THE DIMENSIONS SHALL BE APPROVED BY THE REGIONAL DIRECTOR OR BY HIS/HER DESIGNEE.
7. NYSD-12 MAY BE USED IN PLACE OF NYSD-11.

CHANNELIZING DEVICES

1. WHERE POSSIBLE ALL CHANNELIZING AND GUIDING DEVICES ARE TO BE PLACED SO AS TO PROVIDE A MINIMUM 2' LATERAL CLEARANCE TO THE TRAVELED WAY.

PUBLIC ACCESS

1. PROPERTY OWNERS WHOSE DRIVEWAYS WILL BE MADE IMPASSIBLE SHALL BE NOTIFIED BY THE CONTRACTOR IN WRITING AT LEAST 24 HOURS IN ADVANCE OF THE DRIVEWAY FOR ALL ACCESS PROCEDURES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR RESTORING ALL ACCESS TO ALL DRIVEWAYS AS SOON AS POSSIBLE.
2. SUITABLE RUMPS SHALL BE INSTALLED TO MAINTAIN SMOOTH TRANSITIONS FROM RESIDENTIAL AND COMMERCIAL DRIVEWAYS TO AND FROM THE WORK AREA.

LANE CLOSURES

1. THE CONTRACTOR SHALL LOCATE LANE CLOSURES TO PROVIDE OPTIMUM VISIBILITY, I.E. BEFORE CURVES AND DRETS, TO THE EXTENT CONDITIONS PERMIT.
2. THE ENGINEER MAY REQUIRE THAT ALL LANES BE RE-OPENED AT ANY TIME IF THE ROUTE IS NEEDED FOR EMERGENCY PURPOSES. THIS COULD INCLUDE INCIDENTS AT LOCATIONS OUTSIDE THE CONTRACT LIMITS.

LANE WIDTHS

1. UNLESS AUTHORIZED BY THE ENGINEER, THE MINIMUM LANE WIDTHS FOR WORK ZONE TRAVEL LANES SHALL BE AS FOLLOWS: FREEWAYS AND/OR EXPRESSWAYS IS 11'; THE MINIMUM LANE WIDTH FOR ALL OTHER TYPES OF ROADWAYS IS 10'.
2. THE CONTRACTOR SHALL PROVIDE A WRITTEN NOTICE TO THE ENGINEER, A MINIMUM OF 21 CALENDAR DAYS IN ADVANCE OF PERFORMING ANY WORK THAT RESULTS IN THE REDUCED WIDTH OF ANY DRIVEWAY. THE CONTRACTOR SHALL NOTIFY THE REGIONAL MANAGEMENT ENGINEER IN A WRITTEN MANNER.

BARRIER/SHADOW VEHICLES

1. BARRIER AND SHADOW VEHICLES SHALL BE REQUIRED AS PER STANDARD SHEET TITLED "WORK ZONE TRAFFIC CONTROL LEGENDS AND NOTES".
2. NO WORK ACTIVITY, EQUIPMENT, VEHICLES AND/OR MATERIALS SHALL BE LOCATED BETWEEN THE BARRIER OR SHADOW VEHICLE AND THE ACTIVE WORK AREA ROLL AHEAD DISTANCE.
3. THE CONTRACTOR MAY BE REQUIRED TO PROVIDE A BARRIER VEHICLE IN COMMUNICATION WITH POLICE PRESENCE IN THE WORK ZONE, TO BE INCLUDED IN THE UNIT BID PRICE FOR BASIC WORK ZONE TRAFFIC CONTROL.



STATE OF NEW YORK
DEPARTMENT OF TRANSPORTATION

U.S. CUSTOMARY STANDARD SHEET

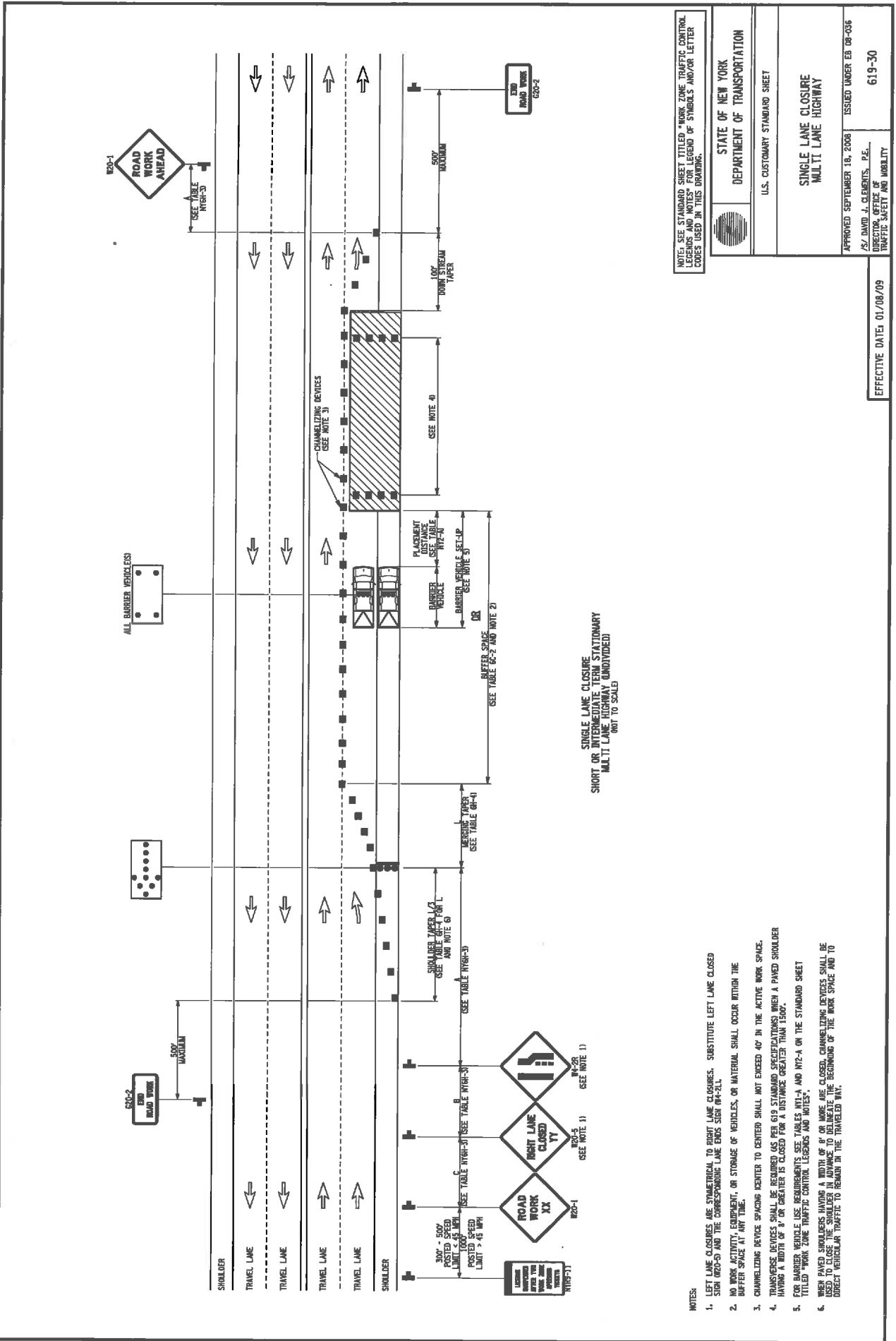
WORK ZONE TRAFFIC CONTROL
GENERAL NOTES

APPROVED SEPTEMBER 18, 2008 ISSUED UNDER EB 08-036

DESIGNED BY
S/ DAVID J. CLEMENTS, P.E.
TRAFFIC SAFETY AND MOBILITY

619-10

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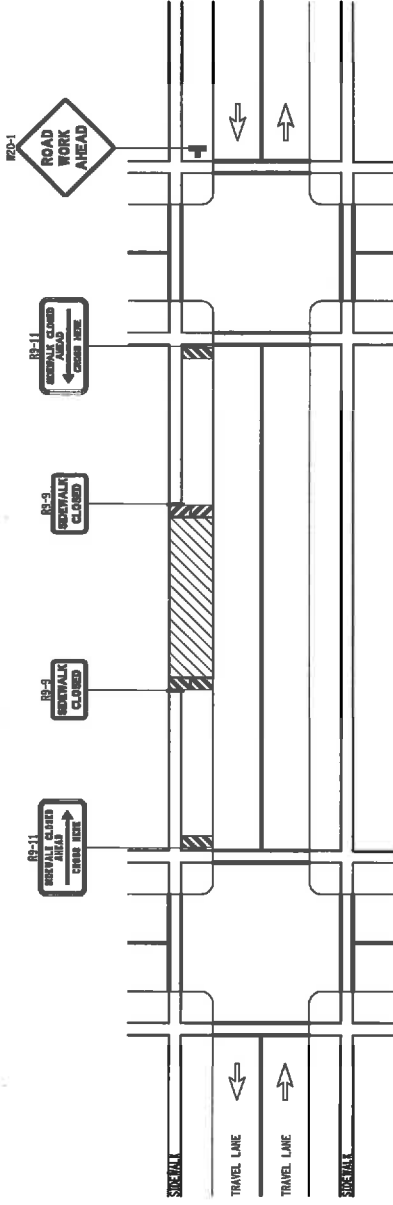


NOTE: SEE STANDARD SHEET TITLED "WORK ZONE TRAFFIC CONTROL LEGENDS AND NOTES" FOR LEGEND OF SYMBOLS AND/OR LETTER CODES USED IN THIS DRAWING.

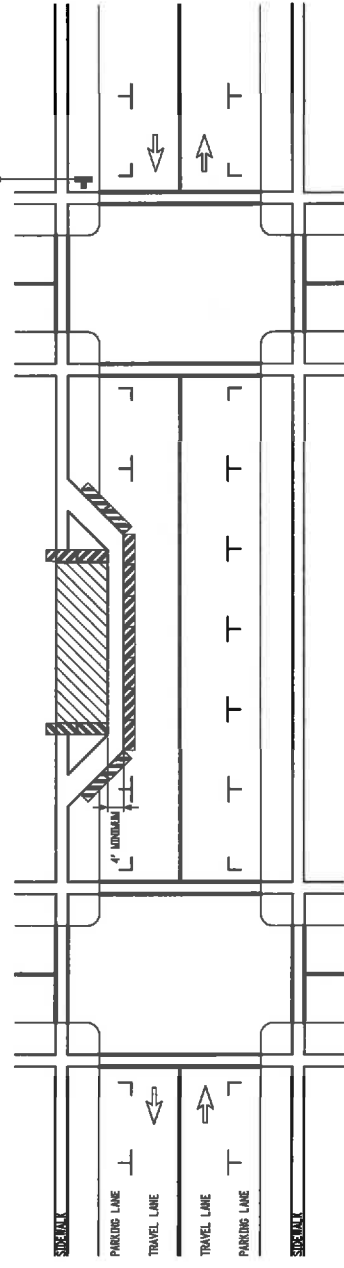
STATE OF NEW YORK
 DEPARTMENT OF TRANSPORTATION
 U.S. CUSTOMARY STANDARD SHEET
 SINGLE LANE CLOSURE
 MULTI LANE HIGHWAY
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 AS/DANIEL J. CLEMENTS, P.E.
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SINGLE LANE CLOSURE
 SHORT OR INTERMITTENT TERM, STATIONARY
 MULTI LANE HIGHWAY
 (NOT TO SCALE)

- NOTES:
1. LEFT LANE CLOSURES ARE SYMMETRICAL TO RIGHT LANE CLOSURES. SUBSTITUTE LEFT LANE CLOSED SIGN (W1R-2) AND THE CORRESPONDING LANE CLOSED SIGN (W1R-1).
 2. NO WORK ACTIVITY, EQUIPMENT, OR STORAGE OF VEHICLES, OR MATERIAL SHALL OCCUR WITHIN THE BUFFER SPACE AT ANY TIME.
 3. CHANNELIZING DEVICE SPACING CENTER TO CENTER SHALL NOT EXCEED 40' IN THE ACTIVE WORK SPACE. TRANSVERSE DEVICES SHALL BE REQUIRED AS PER 619 STANDARD SPECIFICATIONS) WHEN A PAVED SHOULDER HAVING A WIDTH OF 8' OR GREATER IS CLOSED FOR A DISTANCE GREATER THAN 1500'.
 4. FOR BARRIER VEHICLE USE REQUIREMENTS SEE TABLES W1R-4 AND W1R-5 ON THE STANDARD SHEET TITLED "WORK ZONE TRAFFIC CONTROL LEGENDS AND NOTES".
 5. WHEN PAVED SHOULDER HAVING A WIDTH OF 8' OR GREATER ARE CLOSED, CHANNELIZING DEVICES SHALL BE USED TO CLOSE THE SHOULDER IN ADVANCE TO DELINEATE THE BEGINNING OF THE WORK SPACE AND TO DIRECT VEHICULAR TRAFFIC TO REMAIN IN THE TRAVELED WAY.




SIDEWALK DETOUR
SHORT, INTERMEDIATE, OR LONG TERM STATIONARY
2-LANE, 2-WAY ROADWAY
(NOT TO SCALE)



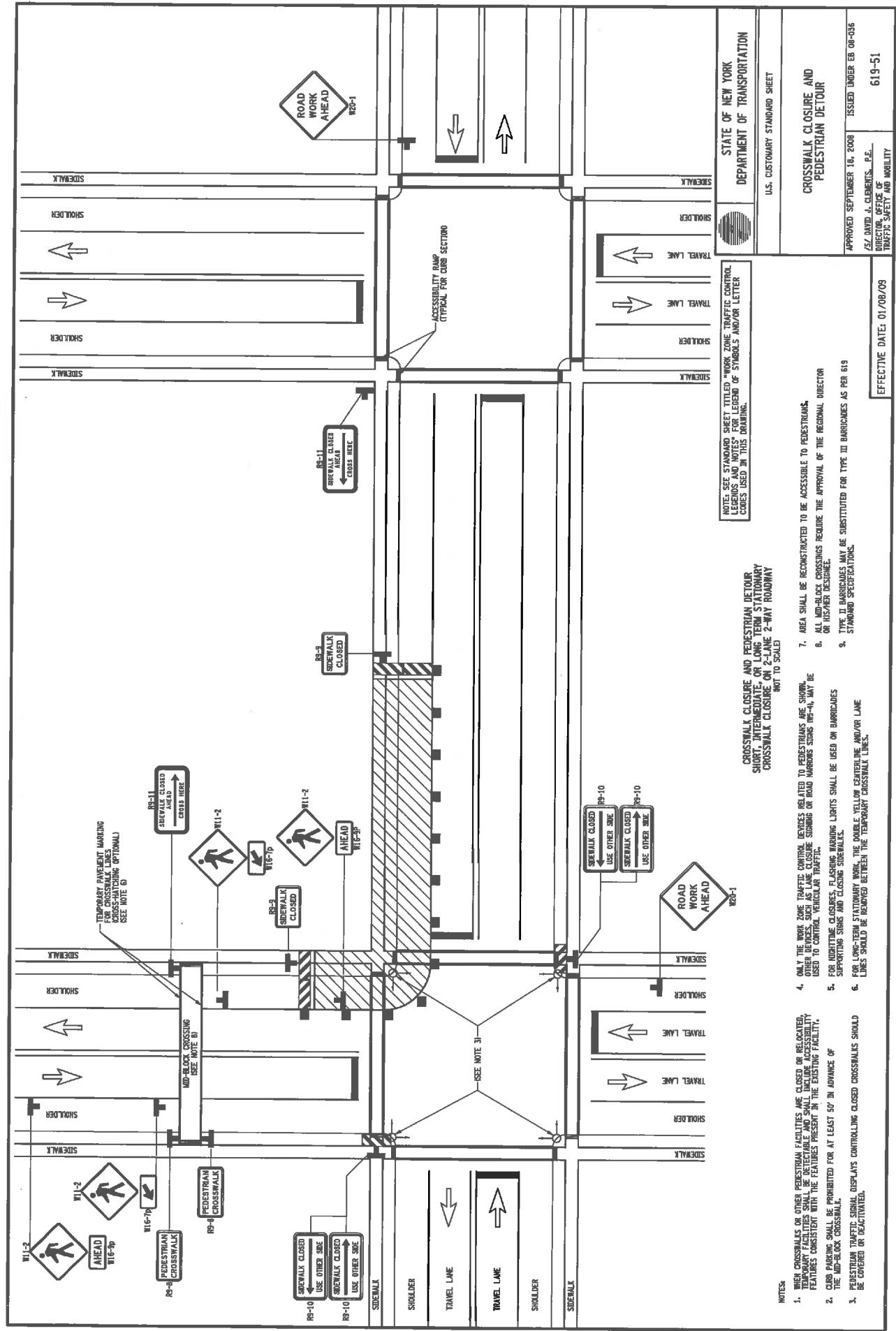
SIDEWALK DIVERSION
SHORT, INTERMEDIATE, OR LONG TERM STATIONARY
2-LANE, 2-WAY ROADWAY
(NOT TO SCALE)

- NOTES
1. NEW PROGRAMS OR OTHER PREVIOUSLY EXISTING ARE PLACED OR BE LOCATED. TEMPORARY FACILITIES SHALL BE IDENTIFIABLE AND SHALL INCLUDE ACCESSIBILITY FEATURES CONSISTENT WITH THE FEATURES PRESENT IN THE EXISTING FACILITY.
 2. THESE SIGN SYSTEMS ARE ANTICIPATED, A TEMPORARY TRAFFIC BARRIER AND TEMPORARY IMPACT ATTENUATOR SHOULD BE USED TO SEPARATE THE TEMPORARY SIDEWALKS FROM VEHICULAR TRAFFIC.
 3. ONLY THE WORK ZONE TRAFFIC CONTROL DEVICES RELATED TO PEDESTRIANS ARE SHOWN. OTHER DEVICES, SUCH AS LINE CLOSURE SIGNING OR ROAD NARROWS SIGNS (R4-14), MAY BE USED TO CONTROL VEHICULAR TRAFFIC.
 4. FOR NIGHTTIME CLOSURES, FLASHING WARNING LIGHTS SHALL BE USED ON BARRICADES SUPPORTING SIGNS AND CLOSING SIDEWALKS.
 5. SIGNS SUCH AS KEEP RIGHT LEFTS SHALL BE PLACED ALONG A TEMPORARY SIDEWALK, WHERE APPLICABLE AND ACCORDING TO AMERICAN WITH DISABILITIES STANDARDS, TO GUIDE OR DIRECT PEDESTRIANS.
 6. TYPE II BARRICADES MAY BE SUBSTITUTED FOR TYPE III BARRICADES AS PER 619 STANDARD SPECIFICATIONS.

NOTE: SEE STANDARD SHEET TITLED "WORK ZONE TRAFFIC CONTROL DEVICES" FOR NOTES FOR LEGEND OF SYMBOLS AND/OR LETTER CODES USED IN THIS DRAWING.

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U.S. CUSTOMARY STANDARD SHEET

CROSSWALK CLOSURE AND
PEDESTRIAN DETOUR

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/S/ DAVID J. CLORENTS, P.E.
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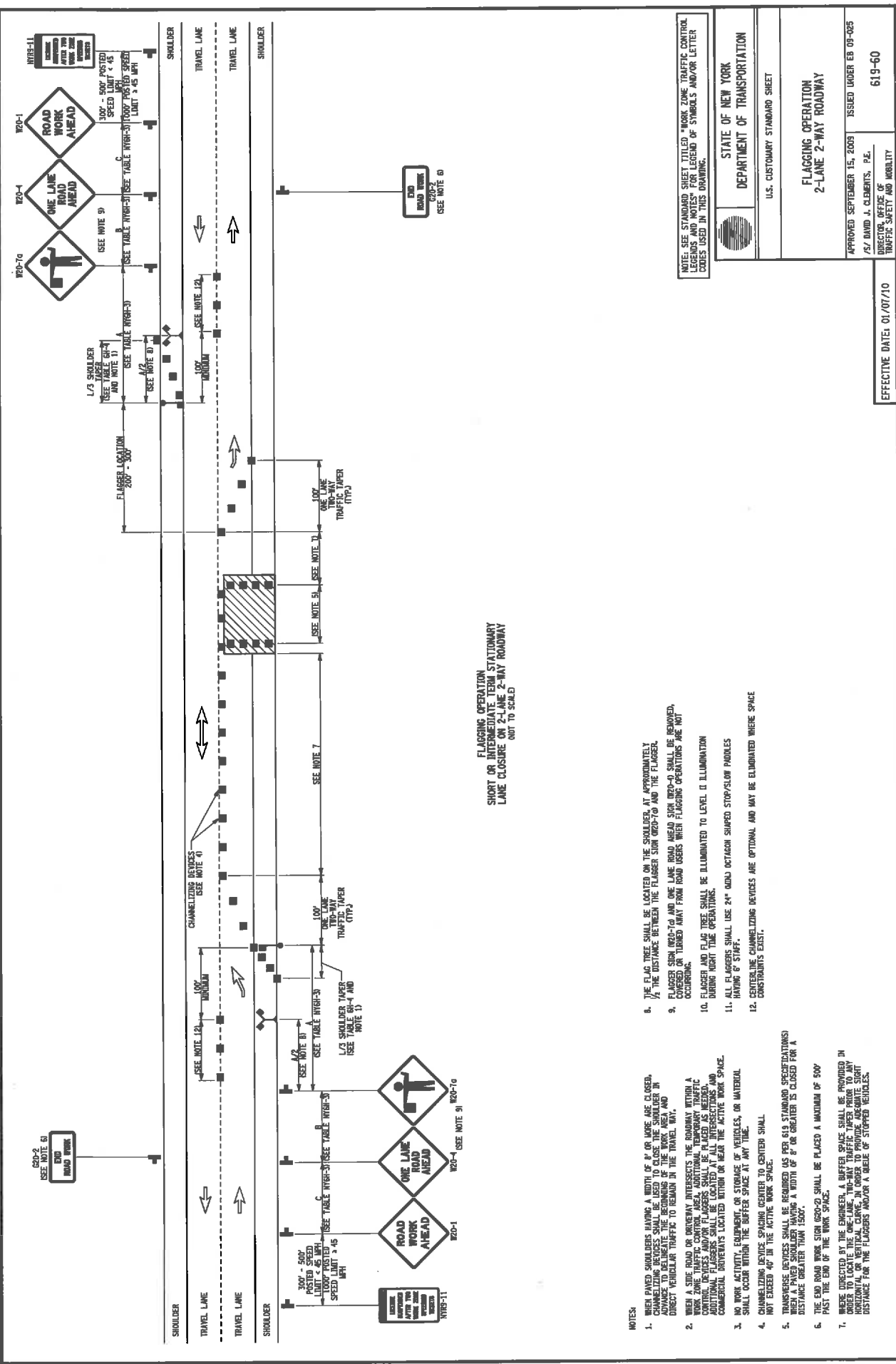
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NOTE: SEE STANDARD SHEET TITLED "WORK ZONE TRAFFIC CONTROL LEGENDS AND NOTES" FOR LEGEND OF SYMBOLS AND/OR LETTER CODES USED IN THIS DRAWING.

CROSSWALK CLOSURE AND PEDESTRIAN DETOUR
SHORT, INTERMEDIATE, OR LONG TERM STATIONARY
CROSSWALK CLOSURE ON 2-LANE 2-WAY ROADWAY
NOT TO SCALE

1. ONLY THE WORK ZONE TRAFFIC CONTROL DEVICES RELATED TO PEDESTRIANS ARE SHOWN. OTHER DEVICES SUCH AS TRAFFIC SIGNALS, SIGNS OR ROAD MARKING SIGNS (M-31), MAY BE USED TO CONTROL VEHICULAR TRAFFIC.
2. CURB PARKING SHALL BE PROHIBITED FOR AT LEAST 50' IN ADVANCE OF THE MID-BLOCK CROSSWALK.
3. PEDESTRIAN TRAFFIC SIGNAL DISPLAYS CONTROLLING CLOSED CROSSWALKS SHOULD BE COVERED OR DEACTIVATED.
4. WHEN CROSSWALKS OR OTHER PEDESTRIAN FACILITIES ARE CLOSED OR RELOCATED, TEMPORARY FACILITIES SHALL BE DETECTABLE AND SHALL INCLUDE ACCESSIBILITY FEATURES CONSISTENT WITH THE FEATURES PRESENT IN THE EXISTING FACILITY.
5. SUPPORTING SIGNS AND CLOSING SIDEWALKS.
6. FOR LONG-TERM TEMPORARY WORK, THE PANELS, YELLOW CENTERLINE AND/OR LANE LINES SHOULD BE REMOVED BETWEEN THE TEMPORARY CROSSWALK LINES.
7. AREA SHALL BE RECONSTRUCTED TO BE ACCESSIBLE TO PEDESTRIANS.
8. ALL MID-BLOCK CROSSINGS REQUIRE THE APPROVAL OF THE REGIONAL DIRECTOR OR HIS/HER DESIGNEE.
9. TYPE II BARRICADES MAY BE SUBSTITUTED FOR TYPE III BARRICADES AS PER 613 STANDARD SPECIFICATIONS.

- NOTES
1. WHEN CROSSWALKS OR OTHER PEDESTRIAN FACILITIES ARE CLOSED OR RELOCATED, TEMPORARY FACILITIES SHALL BE DETECTABLE AND SHALL INCLUDE ACCESSIBILITY FEATURES CONSISTENT WITH THE FEATURES PRESENT IN THE EXISTING FACILITY.
 2. CURB PARKING SHALL BE PROHIBITED FOR AT LEAST 50' IN ADVANCE OF THE MID-BLOCK CROSSWALK.
 3. PEDESTRIAN TRAFFIC SIGNAL DISPLAYS CONTROLLING CLOSED CROSSWALKS SHOULD BE COVERED OR DEACTIVATED.



**FLAGGING OPERATION
SHORT OR INTERMEDIATE TERM STATIONARY
LANE CLOSURE ON 2-LANE 2-WAY ROADWAY
(NOT TO SCALE)**

- NOTES:**
- WHEN PAVED SHOULDERS HAVING A WIDTH OF 8' OR MORE ARE CLOSED, CHANNELIZING DEVICES SHALL BE USED TO CLOSE THE SHOULDER IN BOTH DIRECTIONS TO PREVENT TRAFFIC FROM ENTERING THE SHOULDER AND TO DIRECT VEHICULAR TRAFFIC TO REMAIN IN THE TRAVEL WAY.
 - WHEN A SIDE ROAD OR DRIVEWAY INTERSECTS THE ROADWAY WITHIN A WORK ZONE, TRAFFIC CONTROL DEVICES AND/OR TEMPORARY TRAFFIC CONTROL DEVICES AND/OR FLAGGERS SHALL BE PLACED AS NEEDED. ADDITIONAL FLAGGERS SHALL BE LOCATED AT ALL INTERSECTIONS AND COMMERCIAL VEHICLES LIGHTED WITHIN OR NEAR THE ACTIVE WORK SPACE.
 - NO WORK ACTIVITY, EQUIPMENT, OR STORAGE OF VEHICLES, OR MATERIAL SHALL OCCUR WITHIN THE BUFFER SPACE AT ANY TIME.
 - CHANNELIZING DEVICE SPACING CENTER TO CENTER SHALL NOT EXCEED 40' IN THE ACTIVE WORK SPACE.
 - TRANSVERSE DEVICES SHALL BE REQUIRED US PER 613 STANDARD SPECIFICATIONS) WHEN A PAVED SHOULDER HAVING A WIDTH OF 8' OR GREATER IS CLOSED FOR A DISTANCE GREATER THAN 1500'.
 - THE END ROAD WORK SIGN (G20-2) SHALL BE PLACED A MAXIMUM OF 500' PAST THE END OF THE WORK SPACE.
 - WHERE DIRECTED BY THE ENGINEER, A BUFFER SPACE SHALL BE PROVIDED IN ORDER TO LOCATE THE ONE-LANE, TWO-WAY TRAFFIC TAPER BEHIND TO ANY TRAFFIC SIGNALS, INTERSECTIONS, OR OTHER FEATURES. THE BUFFER SPACE DISTANCE FOR THE FLAGGERS AND/OR A QUEUE OF STOPPED VEHICLES.

- THE FLAG TREE SHALL BE LOCATED ON THE SHOULDER, AT APPROXIMATELY 1/2 THE DISTANCE BETWEEN THE FLAGGER SIGN (W20-70) AND THE FLAGGER.
- FLAGGER SIGN (W20-70) AND ONE LANE ROAD AHEAD SIGN (W20-2) SHALL BE REMOVED, COVERED OR TURNED AWAY FROM ROAD USERS WHEN FLAGGING OPERATIONS ARE NOT OCCURRING.
- FLAGGER AND FLAG TREE SHALL BE ILLUMINATED TO LEVEL II ILLUMINATION DURING NIGHT TIME OPERATIONS.
- ALL FLAGGERS SHALL USE 24" ROUND OCTAGON SHAPED STOP/SLOW PADLOCKS HAVING 8" STAFF.
- CENTERLINE CHANNELIZING DEVICES ARE OPTIONAL, AND MAY BE ELIMINATED WHERE SPACE CONSTRAINTS EXIST.

STATE OF NEW YORK
DEPARTMENT OF TRANSPORTATION

U.S. CUSTOMARY STANDARD SHEET

**FLAGGING OPERATION
2-LANE 2-WAY ROADWAY**

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